

ARCHITECTURAL DESIGN STANDARDS

APRIL
2020





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1.0 Introduction

The Red Deer Regional Airport is located at the heart of the Camex Economic Corridor between Edmonton and Calgary, with access to approximately 3 million people within a 90 minute driving radius. The Airport is on the door- step of the City of Red Deer and the Town of Penhold and adjacent to the residential community of Springbrook. Last year the Airport had approximately 80,000 aircraft movements providing air services in central Alberta.

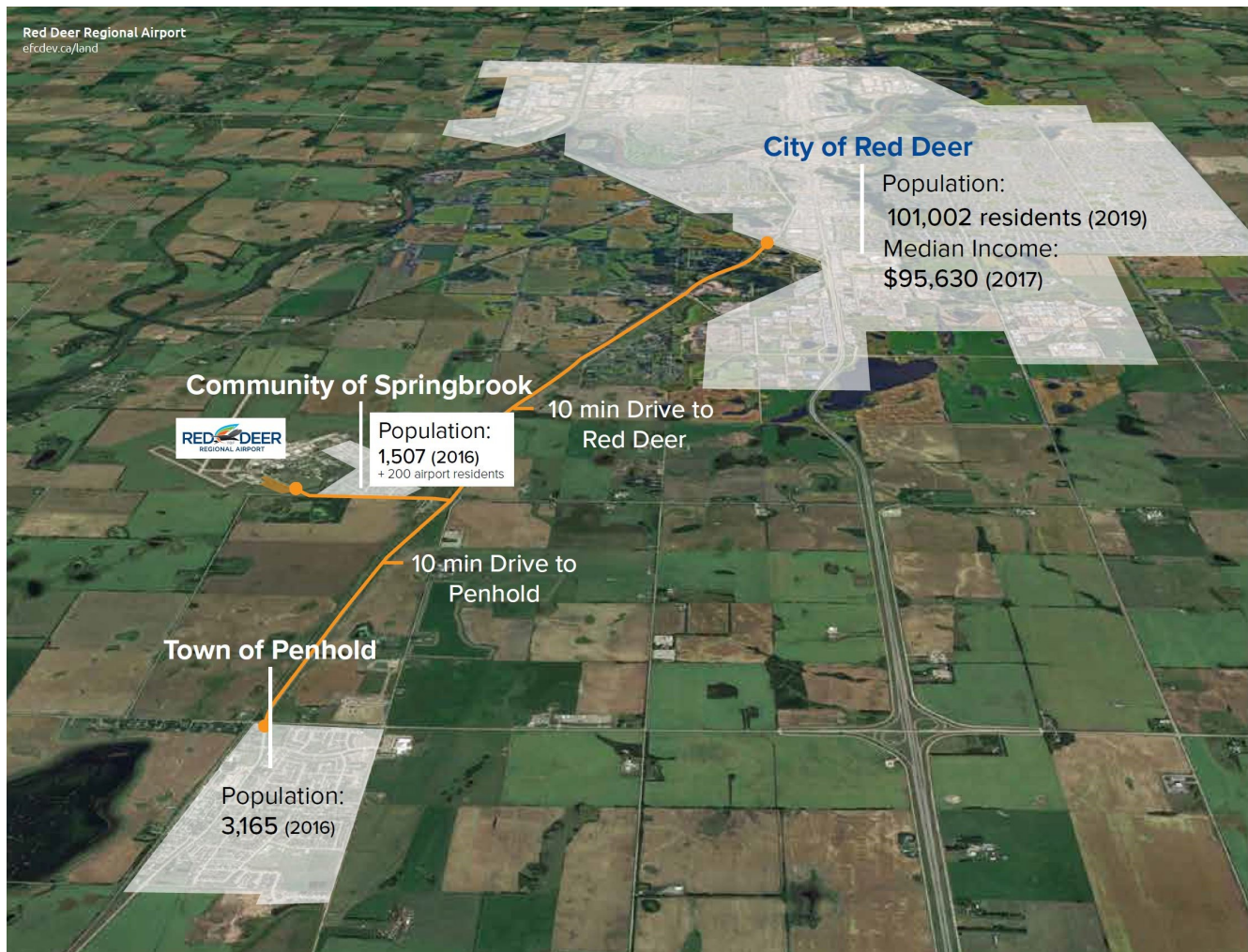


Figure 1.0 (Location Map)

The future of the Red Deer Regional Airport is growth. It is one of the busiest regional airports in Canada and the 4th busiest Airport in Alberta. It is home to an active business park with access to over 800,000 m² of air side real estate, including two flying schools and two aerial firefighting companies. Approximately 250 people work at the Airport which generates a significant economic impact in the Red Deer sub-region.

The purpose of the Red Deer Regional Airport *Architectural Design Guidelines* is to provide a consistent base for design and development decisions as the Airport grows.

2.0 Goals

- Promote the Airport and the Red Deer sub-region
 - Aid developers, designers and planners deliver creative development and design solutions
 - Create an identifiable/unified sense of place for the Airport
 - Ensure a quality-built environment by enforcing high building standards
 - Create attractive and marketable lease space
 - Ensure new and existing development is compatible
 - Attract complementary development to each airport functional area
 - Continue Airport operations in harmony with the community of Springbrook and Red Deer County
 - Achieve the desired aesthetic theme by re-inventing the historic military base character in new development and entry features
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3.0 Development Procedures

3.1 Development Application Process

a) Application Submission

A development application form must be filled out along with detailed drawings outlining the work that is to be performed. The application shall be sent for review to Red Deer County.

b) Preliminary Review & Referral

Once the application has been submitted and checked for completeness, the preliminary review will begin. The project will be circulated to those parties that may be affected, such as the Red Deer Regional Airport Board.

c) Compliance with other Governing Authorities

It is the Applicant's consultants and contractor's responsibility to ensure that all applicable codes and their most current regulations are adhered to, and that the appropriate permit(s) are acquired. For instance, Transport Canada and NAV CANADA may require the applicant to prepare a development area site analysis.

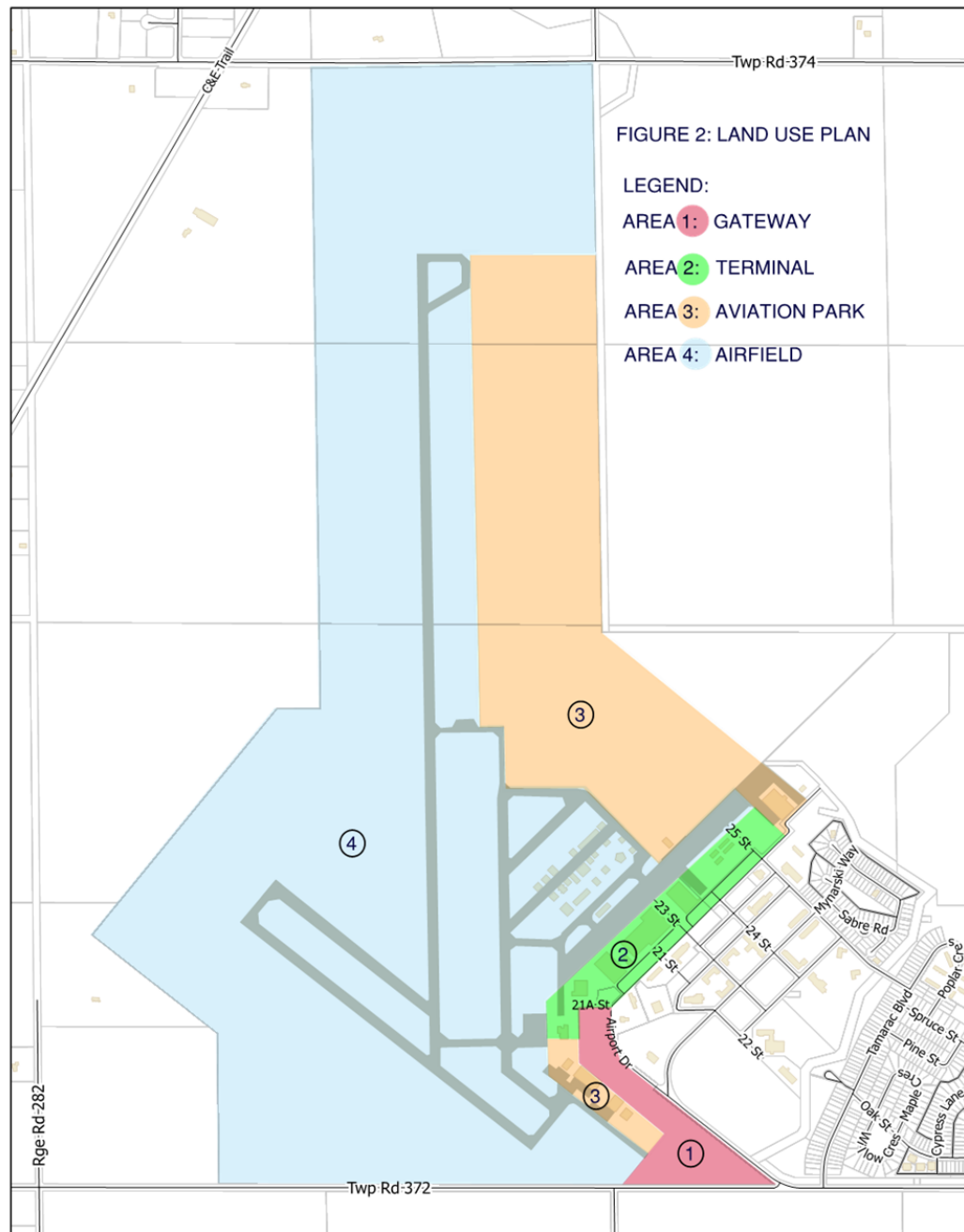
3.2 Building Permit Requirements

Applicants intending to undertake construction at the Red Deer Regional Airport require Building Permits issued by Red Deer County. Upon issuance of the Building Permits, the Applicant shall provide a copy of the approved Building Permit to the Red Deer Regional Airport.

4.0 Land Use Plan

4.1) Introduction

The Red Deer Regional Airport embraces and celebrates its history and imagery as a former military base. The Airport would like to recall this imagery and re-invent it into a modern, vibrant 21st century airport. The following *Architectural Design Guidelines* will apply style, materials, massing, form, entry, color, decoration, scale, rooflines, parking and landscaping thoughts to achieve the desired theme and overall aesthetic to the built environment.



The key “aesthetic” issue is the overall appearance of the Red Deer Regional Airport to the existing and prospective businesses and to the adjacent residential community. By nature, airports typically take on an “industrial” character. Industrial buildings generally utilize relatively inexpensive materials (e.g. metal cladding/ concrete block) and unsightly outdoor storage areas are expected. The Red Deer Regional Airport wants to achieve a high-quality built environment that is visually and economically attractive.

4.2) Land Use Functional Areas

The Red Deer Regional Airport has four distinctive land use functional areas, three of which merit careful consideration with regards to site and building design.

Figure 2.0 (Land Use Functional Areas)

Gateway Functional Area 1

This area is composed of largely undeveloped lands which front on to the southwest face of Airport Drive and are partially visible from Highway #2A and the adjacent residential community of Springbrook. Complementary uses would be airport related offices, retail services, hotels, restaurants, museums, public/semi-public, emergency/protective services, (refer to the Red Deer County Land Use Bylaw).

All buildings should be attractive in appearance and shall be constructed of materials and construction methods that comply with the Safety Codes Act. Prospective tenants and their designers are required to develop sites and buildings which enhance these areas rather than detract from the surrounding areas. The highest level of design standards should be implemented in this functional area to enable these developments to reflect the “face” of the Airport.

Architectural Style

The architectural style in this area should incorporate the historic military base theme in some manner and buildings and the landscape within this area should be implemented to the highest standard on the Airport.



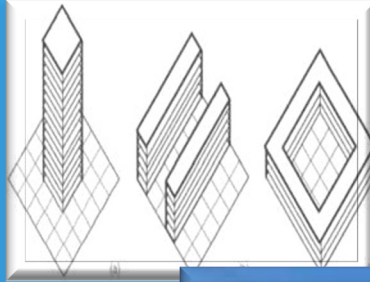
Materials

New development and remodels should use higher grade and multiple types of building materials and finishes on at least 50% of the building to create high quality buildings and visual interest.



Massing

Represents the configuration of the shapes of the major components of a building. These components should match the function and be distinct and different, e.g. a hotel could employ different massing blocks to reflect the different functions and each block could reflect the historic military base building character and create interest.



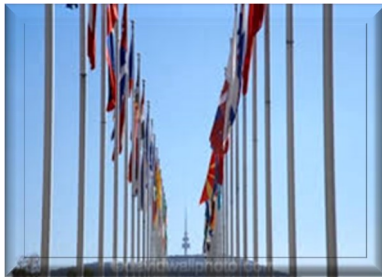
Form

Is the articulation of the mass of the building components with space to create different forms, i.e. regular, irregular, inspired by nature or in this case inspired by the military base/aircraft industry. Buildings in this area should include different roof projections and heights, use different geometric shapes for the different functions of the building, step walls in or out between building components to reflect the historic military base imagery and create visual interest, e.g. a strip mall could be articulated with multiple bay projections and could have shapes that reflect the historic military base character and create interest.



Entry

The primary purpose of development on these lands is to provide gateway buildings and feature(s) to announce that you are entering the Airport. These buildings and features should be located on the street façade on Airport Drive and are to be architecturally designed to create a sense of entrance and a strong historic military airport/corporate image. Buildings themselves could be designed as a gateway features e.g. the Dulles Airport in Washington, the building design concept is based on an airplane's wing. Other elements including gates, sculpture, boulevard tree plantings, gateway signage, flags and mounted display planes are all examples of gateway features which can be used to create a visually dominate "face" for the Airport.



Color

Should conform to the Airport's approved color palette to enhance and complement the overall image of the Airport's historic military base character and create interest e.g. light beige primary, light grey complementary and green accent represents one color



Decoration

Are elements used in a design, or a motif, other than an architectural feature; installed, attached, painted or applied to the exterior of a building or structure for the purpose of ornamentation or artistic expression. Decorative elements should be used on building facades in this area to reflect the historic military base character and to create interest.



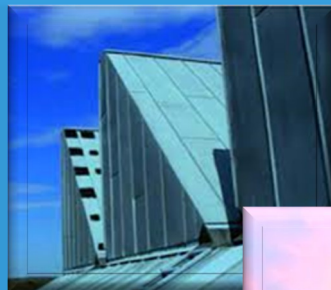
Scale

Buildings or portions of buildings that feature human related services should reflect a human scale and those portions of a building that relate to other functions should reflect that function in the building's scale, e.g. doorways transition from the larger building scale to a human scale.



Rooflines

Buildings should have variations, overhangs and roof shapes to reflect the airport's functional and historic military base character and create interest



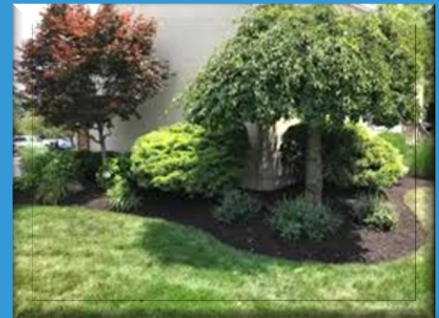
Parking

And maneuvering areas shall be paved to the Airport's engineering standards and the use of paving brick, special paving or sidewalk treatments should be required on 15% of the hard-surfaced areas.



Landscaping

Landscaped areas should be located in the front yards adjacent to Airport Drive and other streets requiring one (1) tree for every 35 m² and one (1) shrub for every 40m² creating a landscaped site coverage of a minimum of 30%. The Site planting plan should incorporate landscaping modules of three (3) deciduous trees, two (2) coniferous trees and ten (10) shrubs and meet the minimum standards of the Red Deer County Land Use Bylaw.



Terminal Functional Area 2

The terminal lands are located on the main access road into the airport (Airport Drive) and are composed of lands which are largely developed with the terminal and adjacent primary air service tenant's hangers and service shops, e.g. Buffalo, Air Spray, etc. If this key corridor is developed appropriately, it should enhance rather than detract from the surrounding areas. Complementary uses would be airport terminals and ancillary buildings, airport service offices, emergency/protective services, (refer to the Red Deer County Land Use Bylaw).

All buildings should be attractive in appearance and shall be constructed of materials that comply with the Safety Codes Act. Prospective tenants and their designers should develop sites and buildings which will enhance these areas rather than detract from the surrounding areas. A high level of design standards should be implemented for these developments to reflect the "heart" of the Airport.

Architectural Style

There are no restriction on the architectural style; however, buildings within this area should be subject to a high-quality design standard and should incorporate elements of the historic military base character and create interest.



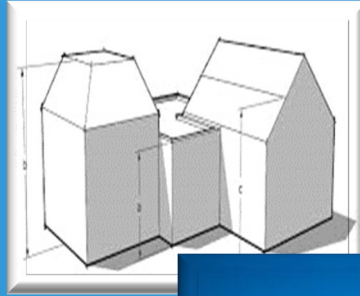
Materials

New development and remodels should use a higher grade of building materials and finishes on at least 30% of the building.



Massing

Represents the shapes of the major components of a building. These components should match the function and are to be distinct and different, e.g. the massing of the terminal and aircraft related buildings should articulate the use.



Form

Is the articulation of the mass of building components with space to create forms, i.e. regular, irregular, inspired by nature or in our case inspired by the military/aircraft industry. Buildings in this area should include different roof projections and heights, use different geometric shapes for the different portions of the building, step walls in or out between building components to create visual interest, i.e. airport related buildings could be articulated with projections and should have rooflines to reflect the historic military base character and create interest.



Entry

Entrances should be located on the street façade adjacent to Airport Drive and are to be architecturally designed to create a strong airport/corporate image and a sense of entrance, e.g. elements including massing, color, flags, and material changes and/or entrance canopies are all examples of architectural features to create a visually dominate sense of entrance.



Color

Should conform to the Airport's approved color palette to enhance and complement the overall image of the Airport's built environment and create interest e.g. light grey primary, grey complementary and yellow accent represents one color concept.



Decoration

Are elements used in a design, or a motif, other than an architectural feature; installed, attached, painted or applied to the exterior of a building or structure for the purpose of ornamentation or artistic expression. Decorative elements should be used on building facades in this area, to reflect the historic military base character and to create interest.



Scale

Buildings or portions of buildings that relate to human activities and services should use architectural elements to transition down to human scale, e.g. the use of canopy and doorway elements transition from the main component of the building to a human scale.



Rooflines

Buildings should have variations, overhangs and roof shapes to reflect the airport's functional and historic military base character and create interest



Parking

Parking and maneuvering areas shall be paved to the Airport's engineering standards and paving brick and other special paving and sidewalk treatments should be applied on 10% of the hard-surfaced area



Landscaping

Landscaped areas should be located in the front yards adjacent to Airport Drive and other streets requiring one (1) tree for every 40 m² and one (1) shrub for every 50m² creating a landscaped site coverage of a minimum of 25%. The Site planting plan should incorporate landscaping modules of three (3) deciduous trees, two (2) coniferous trees and ten (10) shrubs and meet the minimum standards of the Red Deer County Land Use Bylaw.



Aviation Park Functional Area 3

is composed of lands to the southwest of Functional Area 1/Airport Drive and lands to the North and East of Functional Area 2/Airport Drive which are partially developed and represent future growth lands ready for new development. These areas contain generic aircraft hanger and service shops including the existing Avionics Centre and the Sky Wings Academy which are generally one -two story metal clad buildings. Complementary uses would be airport service-related shops, warehouses, emergency/protective services, training facilities and manufacturing buildings (refer to the Red Deer County Land Use Bylaw).

Despite an “industrial” type character, all buildings should be attractive in appearance and high levels of design and quality are encouraged. Buildings shall be constructed of materials that comply with the Safety Codes Act. Prospective tenants and their designers are encouraged to develop sites and buildings which will enhance these areas rather than detract from the surrounding areas. While a high level of “industrial” design standards are encouraged for these areas, tenants, businesses and builders should consider any actions that will elevate the imagery and overall aesthetics of the “body” of the Airport.

Architectural Style

there is no restriction on the architectural style; however, buildings within this area are encouraged to build high quality developments which will enhance the overall aesthetic appearance of the Airport and consider incorporating the historic military base character and create interest.



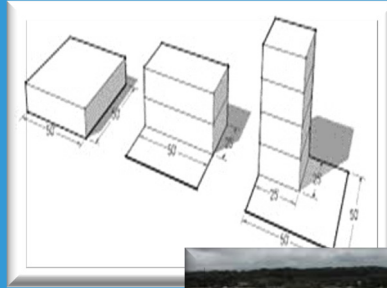
Materials

New development and remodels shall use higher grade building materials and finishes on at least 20% of the building



Massing

Represents the shapes of the major components of a building. The massing of these components should reflect the function and are to be distinct and different, i.e. it is encouraged that the massing of the shop/warehouse be at a larger scale to distinguish the office component



Form

is the articulation of the mass of building components with space to create forms, i.e. regular, irregular, inspired by nature or in our case inspired by the military/aircraft industry. Buildings in this area are encouraged to include different roof projections and heights, use different geometric shapes for the different portions of the building, step walls in or out between building components to create visual interest, e.g. airport related buildings could be articulated with projections and could have roof lines that reflect the historic military base character and create interest.



Entry

Entrances are encouraged to be located on the street façade if possible and are encouraged to be architecturally designed to create a strong airport/corporate image and a sense of entrance, e.g. elements including massing, color, and material changes and/or entrance canopies are all examples of architectural features which can be used to create a visually dominate sense of entrance.



Color

it is encouraged that the buildings in Functional Area 3 conform to the Airport's approved color palette to enhance and complement the overall image of the Airport's built environment and create interest, e.g. blue-grey primary, grey complementary and red accent represents one color concept.



Decoration

are elements used in a design, or a motif, other than an architectural feature; installed, attached, painted or applied to the exterior of a building or structure for the purpose of ornamentation or artistic expression. Decorative elements are encouraged to be used on building facades in this area if appropriate, to reflect the historic military base character and to create interest.



Scale

Buildings or portions of buildings that relate to human activities and services are encouraged to reflect a human scale and those portions of a building that relate to aircraft services are encouraged to transition from human scale to airplane scale.



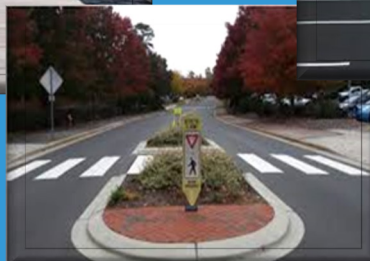
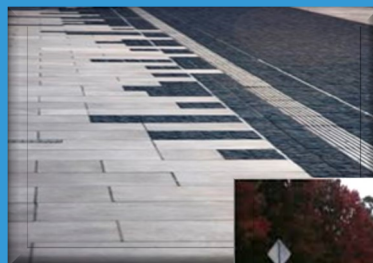
Rooflines

rooflines are encouraged to have variations, overhangs, and roof shapes to reflect the airport's historic character and create interest



Parking

Parking and maneuvering areas shall be paved to the Airport's engineering standards and the use of paving brick or special paving and sidewalk treatments is encouraged where appropriate.



Landscaping

landscaped areas should be located in the front yards adjacent to Airport Drive and other streets requiring one (1) tree for every 50 m² and one (1) shrub for every 60m² creating a landscaped site coverage of a minimum of 20%. The Site planting plan should incorporate landscaping modules of three (3) deciduous trees, two (2) coniferous trees and ten (10) shrubs and meet the minimum standards of the Red Deer County Land Use Bylaw.



Airfield Functional Area 4

Buildings, other than small maintenance and storage sheds, shall not be permitted within the Airfield area. There are no design guidelines for this area. It is encouraged that any structures or storage are kept in a neat orderly manner to enhance the overall aesthetic appearance of the airport.



5.0 Development Guidelines

5.1) Performance Standards

To ensure the safety of airport operations, no development shall cause or create any objectionable condition such as:

- electronic interference
- air contaminants
- earthborn vibrations
- visible emissions
- heat
- particulate emissions
- odors
- toxic materials
- high brightness light sources
- attraction to birds and wildlife
- height obstructions
- excessive noise

5.2) Utilities

To be provided to Tenant on a lot specific basis, connections to existing infrastructure to be provided by the Authorities. Red Deer Regional Airport is a fully serviced area including sanitary, storm, natural gas, telecommunications and power. The Tenant's storm water plan will be reviewed against Red Deer Airport's Storm Water Management Plan.

5.3) Grading

Applicants are required to grade the Site to support and complement the existing drainage system throughout the airport lands. Applicants are required to use satisfactory stormwater management techniques to ensure the stormwater runoff will not adversely affect any adjacent area or pool to attract birds/wildlife.

5.4) Building Height

The height of any Building, including any Building equipment, shall not exceed an elevation that would penetrate air space as defined by Transport Canada or Nav Canada. Building height is limited to 15 m by the Red Deer County Land Use Bylaw with the exception of Aircraft Hangars where Building Heights in excess of 15 m may be allowed as determined by the Authority. Aviation Land Use Regulations have precedence over any other Building Height provisions outlined in these guidelines, where Aviation Land Use Regulations impose a lower height limit.

5.5) Color Palette

The purpose of a color palette is to assist developers and designers choose appropriate color concepts for both new construction and the refinishing of existing buildings. Color is to be used as a major unifying element for the Airport, not only the colors themselves, but their intensity, use and application. The “body” area or the largest mass area of a building, e.g. walls, roof, etc., should have the low- intensity primary color. The next largest mass of a building, i.e. an office on a warehouse, should use a mid-intensity complementary color to the primary color of the “body” of the Building. Accent architectural features, such as windows, doors, fascia, etc. should use a higher intensity color to contrast with the primary and complementary colors. To expedite the design process by providing a pre-approved color palette for buildings in each functional area, see Figure 2 and Appendices.

5.6) Landscaping

To enhance the appearance of each development, all properties shall be landscaped in accordance to the minimum requirements of the Red Deer County Land Use Bylaw. In Functional Areas 1 and 2, Applicants may be required to exceed the minimum requirements that are outlined in the Red Deer County Land Use Bylaw. Applicants will provide a landscape plan for approval with their development application.

5.7) Yards

Yards will be regulated by the Red Deer County Land Use Bylaw. All street frontages shall be treated as front yards. If a development abuts two streets, the Red Deer Regional Airport Authority will designate which street is the street frontage.

5.8) Corner Visibility Triangle

Notwithstanding any other provisions contained in these guidelines, no vegetation, finished lot grade, building or structure may impede visibility within the area defined as the Corner Visibility Triangle in the Red Deer County Land Use Bylaw.

5.9) Vehicle Parking Facilities

Refer to the Red Deer County Land Use Bylaw for the location, minimum number of parking spaces and parking dimensions and handicapped parking requirements. Any parking space or maneuvering areas located in a front yard, side yard or main travel lane shall be paved. Graveled or alternate surface treatments may be allowed on a temporary basis by the Red Deer Regional Airport Authority.

5.10) Loading Spaces

Refer to the Red Deer County Land Use Bylaw for the minimum number of loading spaces and dimensions. Loading spaces must be designed and located so that all vehicles using those spaces can be parked and maneuvered entirely within the bounds of the site before moving onto adjacent streets.

5.11) Outside Storage

Outside storage areas shall not be allowed in front yards and must be screened from view by landscaping or fencing. Outside storage may be allowed to the side or rear of buildings provided that:

- Storage areas do not include any required minimum yards, or required parking or loading spaces and shielded from the public view by an approved method of screening.
- Display areas related to business or industry shall be located on the Site.
- Hazardous material storage must be compliant with applicable Federal and Provincial regulations.

5.12) Refuse Areas

All proposed developments must minimize the visual impact of their refuse areas. Refuse areas should be located within a principal building, or to the side or rear of a principal building, provided that it is not in a required minimum yard clearance or in a required parking or loading space. Applicants are required to visually screen all refuse areas from adjacent sites and streets in a manner that is compatible with the design and external materials of the principal Building on the Site. Refuse containers shall be covered and cleared regularly.

5.13) Exterior Lighting

All exterior lighting must be located, oriented, and shielded to not adversely affect adjacent Sites, or interfere with airport operations. Specific fixture designs or shielding may be required to prevent conflict with airport operations

5.14) Environmental Management

All development proposals within the airport boundary shall comply with any environmental management guidelines established by the Airport Authority. As part of the development process, applicants may be required to submit an environmental construction plan in order for the Airport Authority to review and respond to potential environmental impacts of the proposed development on the airport lands during the planning, construction and operational phases of the development.

5.14) Bird/Wildlife Control

Conflicts between aircraft and birds/wildlife are a major concern to the aviation industry as each year significant dollars are spent to repair and replace damaged aircraft due to bird/wildlife strikes; therefore, no activity or operation that attracts birds/wildlife is permitted on airport lands. It is encouraged that building design or remodeling minimize nesting and perching habitat, e.g. minimize ledges, holes, seal the underside of overhangs, slope surfaces to an angle of 45 degrees and install netting or bird spikes.

6.0 Sign Guidelines

6.1) Traffic Signage

The purpose of traffic signage is to direct traffic and promote safety between aircraft, vehicles and pedestrians. All traffic signage shall conform to the size and location requirements of the Red Deer County Land Use Bylaw and the Red Deer Regional Airport.

6.2) Signage

The purpose of building and site signage is to advertise and inform airport employees and the general public about the Tenant's corporate image, business type and services provided. All Signage shall be complementary to the military base historic character of the surrounding streetscape and the land use functional area. Signage shall be readable from the street and should be illuminated with indirect lighting to enhance visibility at night. The copy area, height, location of building signs, free standing signs, and signs mounted on street light fixtures, street furniture or within the corner visibility triangle; are regulated by the Red Deer County Land Use Bylaw. Any damage to trees or streetscape elements during a sign installation shall be replaced or repaired to the satisfaction of the Red Deer Regional Airport.

Appendices

Functional Area Color Palette



Gateway (Functional Area 1) & Terminal (Functional Area 2)

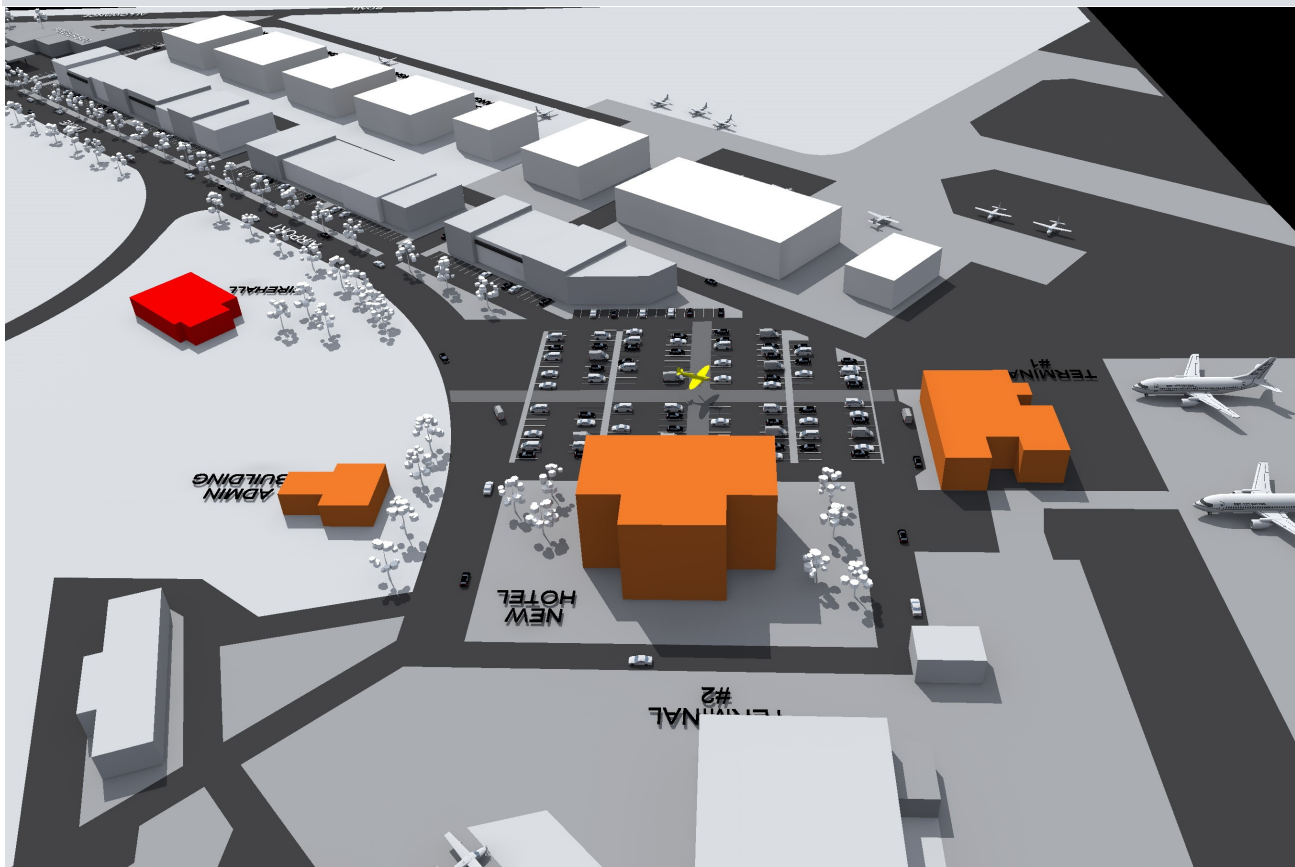
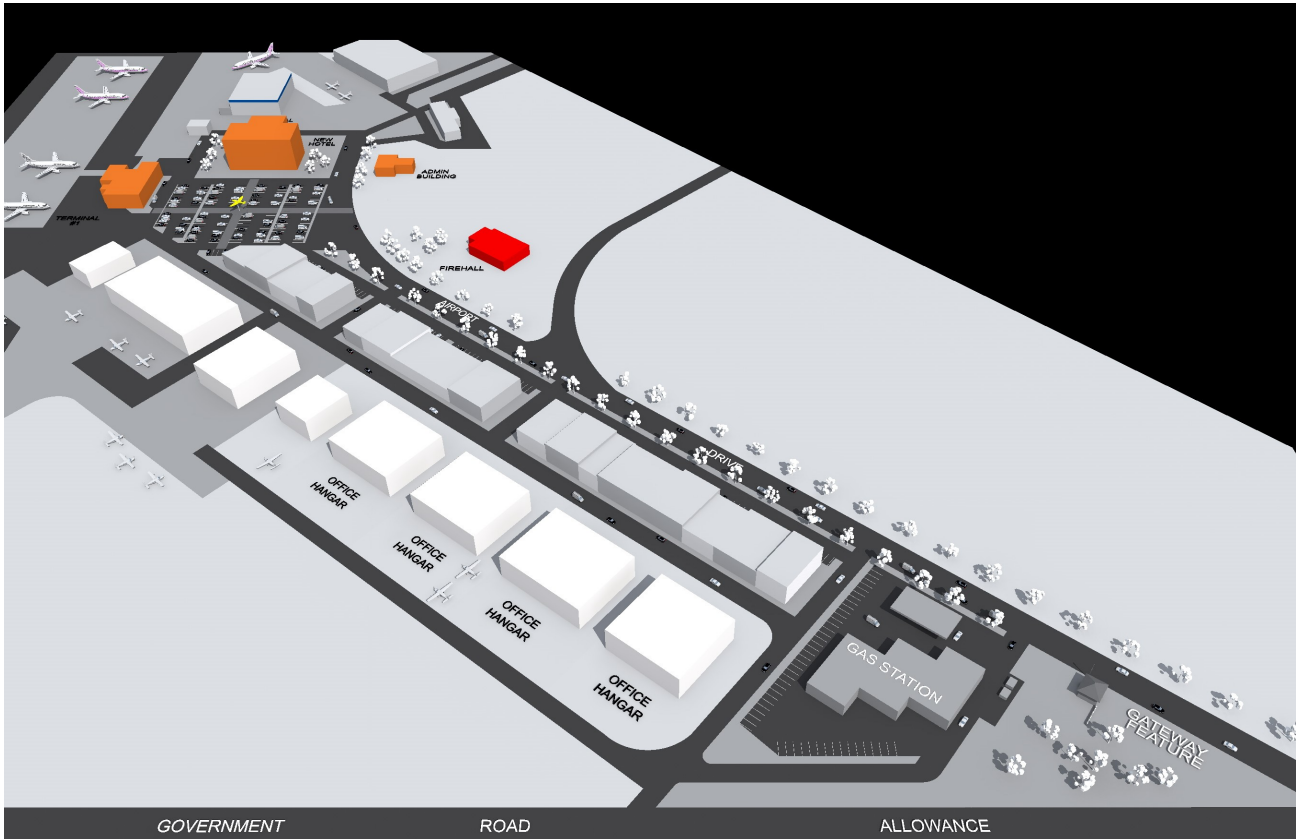


Aviation Park (Functional Area 3)

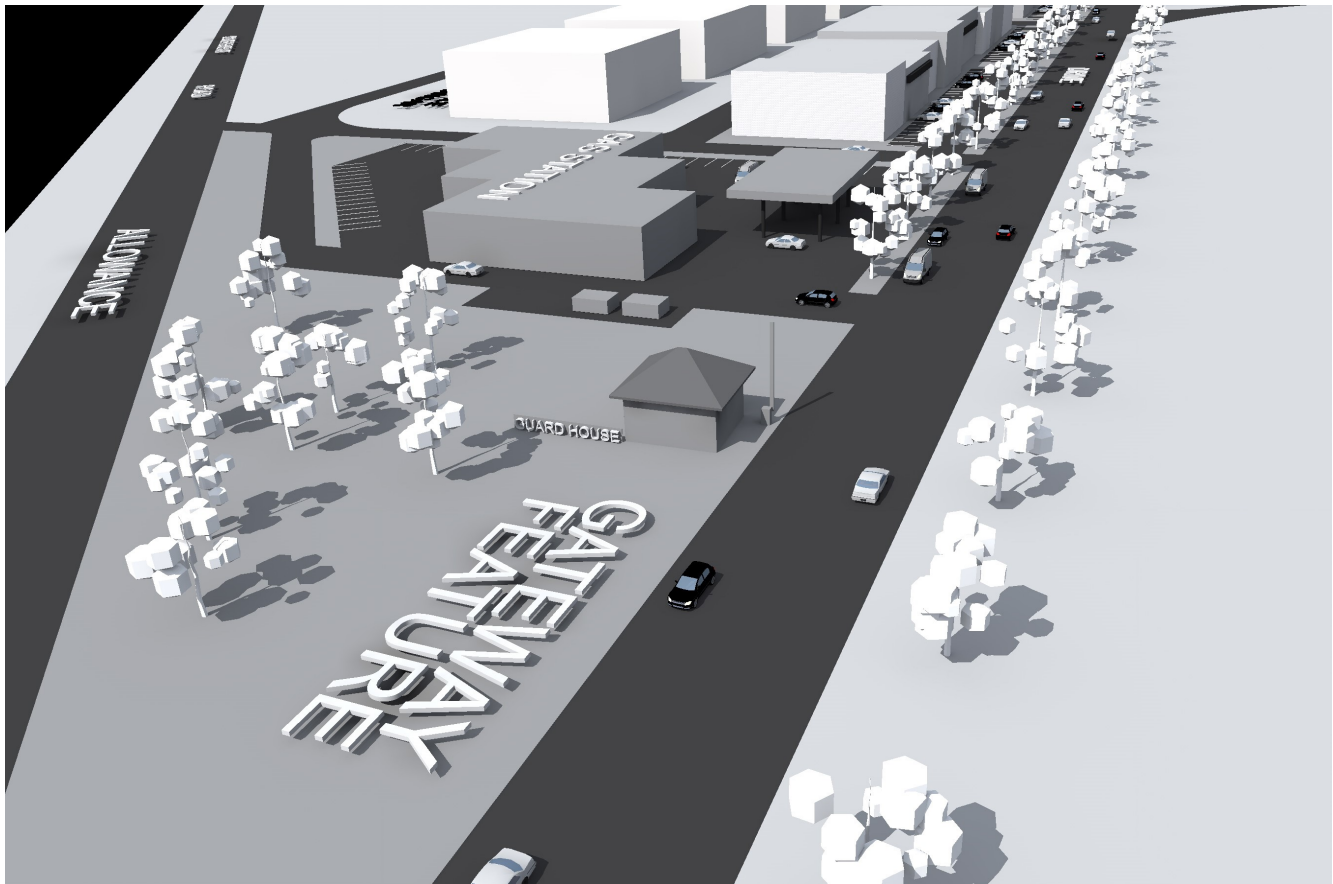
Illustrative Design Examples



Conceptual Development Plan



Conceptual Development Plan



7.0 Definitions

1. “Airport Maintenance and Operational Facilities” means any facility that houses the equipment, materials, or motor vehicles necessary to maintain airport lands, buildings and facilities. This may include garages, workshops, mobile equipment yards, outdoor storage areas, as well as their associated uses such as an office, lunchroom or change room, or overnight accommodations i.e. pilots;
2. “Airport Authority” means the Red Deer Regional Airport;
3. “Apron” means the part of an aerodrome, other than the maneuvering area, intended to accommodate the loading and unloading of passengers and cargo, the refueling, servicing, maintenance and parking of aircraft, and any movement of aircraft, vehicles and pedestrians necessary for such purpose;
4. “Building” includes anything constructed or placed on, in, over or under land but does not include a highway or public roadway, or bridge forming a part of a highway or public road;
5. “Building Height” means that height determined by creating a line parallel to grade along each building elevation and separated vertically from grade by the maximum allowable height for the district. Such line may be exceeded only by part of the building, on no more than one building elevation, and ancillary structures;
6. “Building Permit” means a permit issued pursuant to the Red Deer County Permit By-Law authorizing construction;
7. “Color Palette” means an Airport approved color palette; and buildings are encouraged to use a maximum of three (3) colors; primary, complementary and accent, see Appendices
8. “Corner Visibility Triangle” means a triangular area formed on a corner site by the roadway edge and a straight line that intersects them 7.5 m from the corner where they meet;
9. “Development” means; an excavation or stockpile and the creation of either of them, a Building or an addition to, or replacement or repair of a Building and the construction or placing in, on, over or under land of any of them, a change of use of land or a Building or an act done in relation to land or a Building that results in or is likely to result in a change in the use of the land or Building, a change in the intensity of use of land or a Building or an act done in relation to land or a Building that results in or is likely to result in a change in the intensity of use of the land or Building;
10. “Emergency Response Services” mean development that is required for the protection of persons and property from injury, harm or damage together with incidental storage for emergency vehicles and equipment. Typical uses would include fire stations, police stations, paramedic and security services, bomb disposal facilities, and fire training facilities and other related services;
11. “Essential Airport Services” means those services provided by the Red Deer Regional Airport, the government, contracted by the government (federal, provincial), Nav Canada, and agencies required for safe and efficient operation of the airport, (e.g. fire, police, emergency medical services, hazardous materials);

12. "Fence" means a vertical physical barrier constructed to provide visual screening or to prevent unauthorized access;
13. "Final Utility Service", means water and sewer utilities that are connected to municipal utilities, or that are so connected through the airport utility system, or alternatively, a private water and sanitary treatment system within the Red Deer Regional Airport lands that has been approved by the Red Deer Regional Airport Authority as a final utility servicing. In the case of storm water, this may be in the form of a planned overland stormwater management system;
14. "Frontage" means the width of a lot or a site where it abuts a street including a lane;
15. "Grade" means the elevation of the finished ground surface, excluding an artificial embankment, at any point immediately adjacent the building;
16. "Interim Use" means a use specified as an Interim Use in a Land Use District that may be allowed in the District prior to the installation of final utility services;
17. "Land Use Functional Area" means a district as defined throughout this document;
18. "Land Use Plan" means an approved Red Deer County Land Use Plan;
19. "Landscaped Area" means the portion of a site that is required to be landscaped;
20. "Landscaping" means the modification and enhancement of a site through the use of any or all of the following elements soft landscaping consisting of vegetation such as trees, shrubs, hedges, grass and ground cover. Hard landscaping consisting of non-vegetation materials such as brick, stone, concrete, tile and wood, excluding monolithic concrete and asphalt, architectural elements consisting of wing walls, sculpture and the like;
21. "Lease Line" means the line that defines the boundary of a site that is being leased by the Red Deer Regional Airport or in an interest to another party, or a property line, or the boundaries of the site;
22. "Loading Space" means a space to accommodate a commercial vehicle while it is being loaded or unloaded;
23. "Lot Area" means the area contained within the boundaries of a lot as shown on a plan of subdivision or described in a certificate of title or the boundaries shown on a lease agreement;
24. "Lot Coverage" means that portion of the lot area covered by the principal building, accessory buildings or similar covered structures;
25. "Lot Frontage" means the shortest lot line which abuts a street, other than a bridge, lane, or walkway and in the case of a lot which has two equal lot lines each of which abut a street, other than a bridge, lane, or walkway, means the street to which the lot has the site address;
26. "Lot Width" means the distance between the side property lines of the lot and measured at right angles from the midpoint of the shortest property line;
27. "Outside Storage" means an outside area set-aside for the storage of products, goods or equipment;

28. "Parking Area" means a portion of the land or of a building set aside for parking and maneuvering of motor vehicles;
29. "Primary Access" means the principal means of vehicular entry to and exit from a site or building;
30. "Principal Use" means the main purpose for which a building or site is used;
31. "Property Line" means a legal boundary of an area of land;
32. "Red Deer Regional Airport" means the Authority established pursuant to the "Regional Airports Authorities Act;"
33. "Red Deer County" means the adjacent land lying within the limits of the county;
34. "Screening" means the total or partial concealment of a building, structure or activity by a fence, wall, berm or soft landscaping;
35. "Signs" means any visual medium, including its structure and other component parts, which are used on a permanent or temporary basis to convey information, or to advertise or attract attention to a product, service, place, activity, person, institution, or business, excluding third party advertising;
36. "Site" means an area of land on which a Building or use exists for which a request or application for confirmation and authorization of development is made;
37. "Story" means the space between the top of any floor and the top of the next floor above it, and if there is no floor above it, the portion between the top of the floor and the ceiling above it;
38. "Structure" means anything constructed or erected with a fixed location on the ground or attached to something having a fixed location on the ground and includes walls, light standards, fences and signs;
39. "Use" means the primary function of the site or building on a site;
40. "Utilities" means any of the following; systems for the distribution of gas, whether artificial or natural, electricity, telephone or cable television; facilities for the storage, transmission, treatment, distribution or supply of water; facilities for the collection, treatment, movement or disposal of sanitary sewage.